



## **INTERNAL MAINTENANCE AND REPAIR WORK TO VALLEY TERMINAL BUILDING**

**FRIDAY DRIVE, THREDBO VILLAGE  
KOSCIUSZKO NATIONAL PARK NSW 2625  
LOT 862 DP1128686**

DATE: FEBRUARY 2023

PREPARED FOR: KOSCIUSZKO THREDBO C/- ANDREW HARRIGAN

PREPARED BY: ACCENT TOWN PLANNING PTY LTD

REVISION: 01

## EXECUTIVE SUMMARY

Accent Town Planning has been engaged by Kosciuszko Thredbo Pty Ltd (KT), the owners of Thredbo Resort to prepare the Statement of Environmental Effects (SEE) to complement the Development Application (DA) to the NSW Department of Planning and Environment (DoPE), Alpine Resorts Team, in accordance with the *State Environmental Planning Policy (Precincts – Regional) 2021*, under Part 4 of the *Environmental Planning and Assessment Act 1979* (EP&A Act).

The Statement for which this application applies is for internal maintenance and repair work to the Valley Terminal (VT) building. The purpose of this application is to replace deteriorated internal finishes which will improve the longevity of the building, improve the utility and condition of staff accommodation, whilst maintaining the original alpine character and design of the building.

The proposed development is deemed to be of positive influence on the resort through the provision of an enhanced staff experience.

There are no proposed adverse effects to the local environment, landscape, streetscape, appearance, or scenic quality of the locality. The existing building footprint will be retained in order to minimise the environmental impact of the proposed improvements. The potential impacts to the natural environment are negligible, as the proposed works are internal works only.

The VT building was originally constructed in 1960, currently, the ground floor of the building primarily consists of retail/rentals, office spaces, ski patrol first aid room, take away cafe and public bathrooms. The first floor of the VT Building includes a southern wing that consists of staff accommodation including bedrooms, bathrooms, staff kitchen/dining and Laundry. The northern wing consists of office spaces varying in size, with a small kitchenette and bathrooms.

Currently, the staff accommodation wing of the VT building is in a state of disrepair resulting from its age and use. There are significantly deteriorated areas of ceiling and flooring within staff bathrooms and wet areas that urgently need repair. This application is seeking approval for the removal of deteriorated internal finishes to be replaced with equivalent or improved finishes. Repairs are needed urgently to ensure the condition of the building is improved.

**FIGURE 1 VALLEY TERMINAL SOUTHERN ENTRY**



## 1.0 INTRODUCTION

The project for which this Statement applies is for internal maintenance and repair work to VT building. The VT building is located in Lot 862 DP1128686, Friday Drive, Thredbo within the Kosciuszko National Park. Thredbo Village is located approximately 35km from Jindabyne, within Kosciuszko National Park, NSW.

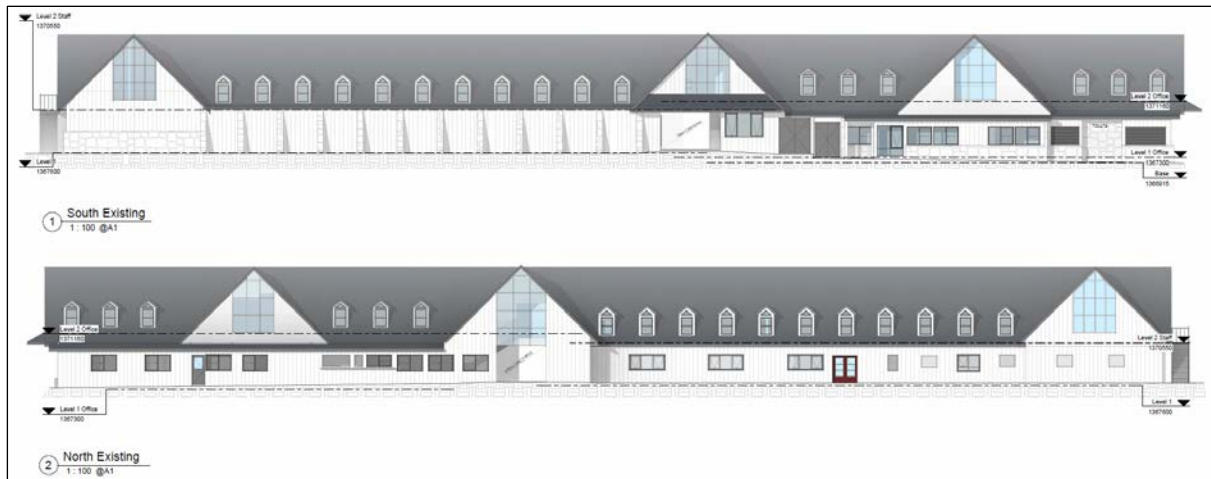
The proposed internal maintenance/repair work will include:

REMOVAL/DEMOLITION	REPAIR/REPLACEMENT	LOCATION
→ Remove carpet and kitchen lino.	→ Like for like	→ Bedrooms → Common area
→ Remove all doors and jambs (Not fire rated)	→ Like for like, new will be fire rated	→ Bedrooms → Kitchen → Laundry
→ Remove all lights, fans, power points and switches	→ Like for like, upgraded to LED lights	→ Bedrooms → Bathrooms → Landry → Common area
→ Remove all toilets, sinks, vanities, shower heads and taps	→ Like for like, upgraded	→ Bathrooms
→ Remove kitchen cabinets sinks and ovens	→ Like for like, upgraded	→ Common area
→ Remove kitchen wall (asbestos)	→ Gyprock Fyrchek MR	→ Common area
→ Remove all tiles. Wet area walls are asbestos removing tiles will damage sheets so removing wall sheet with tiles still attached may be necessary (waterproofing has failed).	→ Gyprock Fyrchek MR	→ Bathrooms
→ Remove all asbestos products (walls, ceilings) all necessary steps must be taken for removal	→ Gyprock Fyrchek MR	→ Bathrooms → Bedrooms → Laundry → Common area → Retail shop
→ Remove all Gyprock (walls & ceilings)	→ Gyprock Fyrchek	→ Bathrooms → Bedrooms → Laundry → Common area → Retail shop
→ Remove all electrical (distribution board very old hard to get replacement parts)	→ Like for like, upgraded	→ Staff storage room (left HS living room in common area) → Retail
→ Remove all plumbing (no isolation valves)	→ Like for like, upgraded	→ Laundry → Bathrooms → Common area
→ Remove fire sprinkler system (to replace with an upgraded modern system)	→ Like for like, upgraded	→ Laundry → Bedrooms → Bathrooms → Common area → Retail shop
→ Remove Subfloor in wet areas (Leaks downstairs)	→ Hardie panel compressed sheet	→ Bathrooms

The internal repair and maintenance works will improve the condition of the VT building and will therefore improve its longevity. The proposed works are consistent with the character and built environment of Thredbo Village.



**FIGURE 5 NORTH AND SOUTH ELEVATION**



**FIGURE 6 EAST AND WEST ELEVATION**





## 2.0 SITE LOCATION & CONTEXT

### 2.1 SITE ANALYSIS

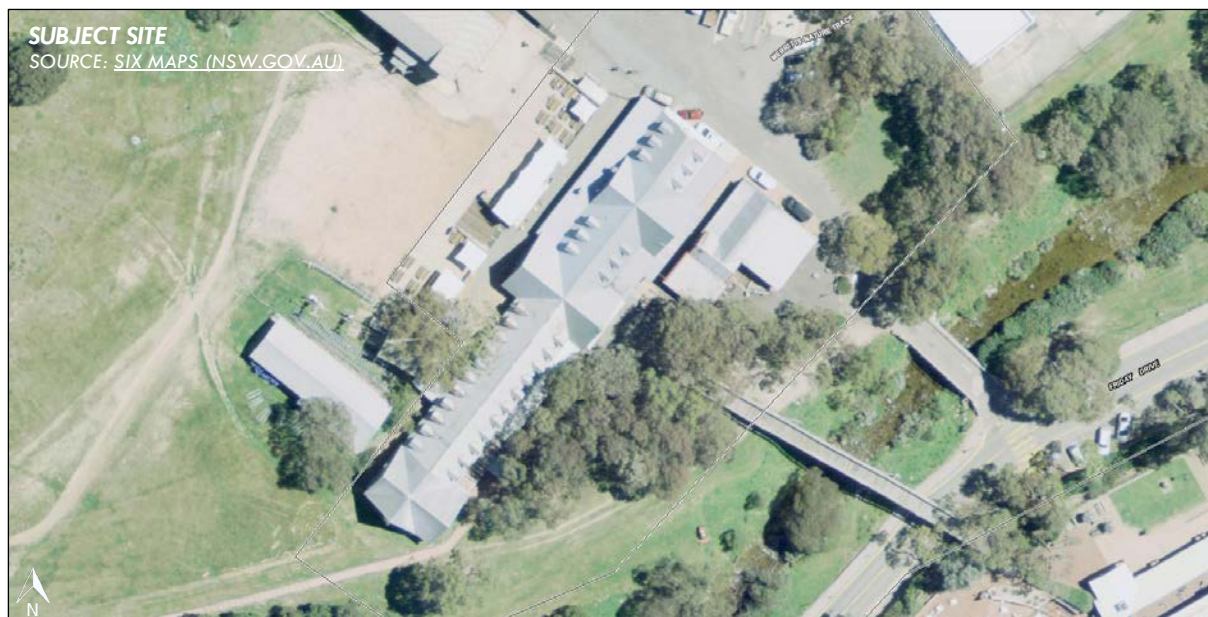
The site is located at Lot 862 DP1128686 Friday Drive, Thredbo within the Kosciuszko National Park. The resort areas, including Thredbo are important to New South Wales due to their regional economic and social contribution as well as their location within a unique alpine environment. Thredbo is located approximately 35kms from the township of Jindabyne.

The Snowy Mountains is a popular tourist destination for both Australian and international travellers. Kosciuszko National Park offers an array of attractions including, Alpine Resorts, world class mountain biking and Australia's highest mountain, Mt Kosciuszko. The local economy is driven by the winter season with snow sports being unique to this region of Australia. A growing summer tourism trade has been developing with activities such as hiking, fishing, kayaking, and mountain biking encouraging outdoor enthusiasts to visit the region year-round.

**FIGURE 7 AERIAL VIEW OF SUBJECT SITE**



**FIGURE 8 CLOSE UP VIEW OF SUBJECT SITE**

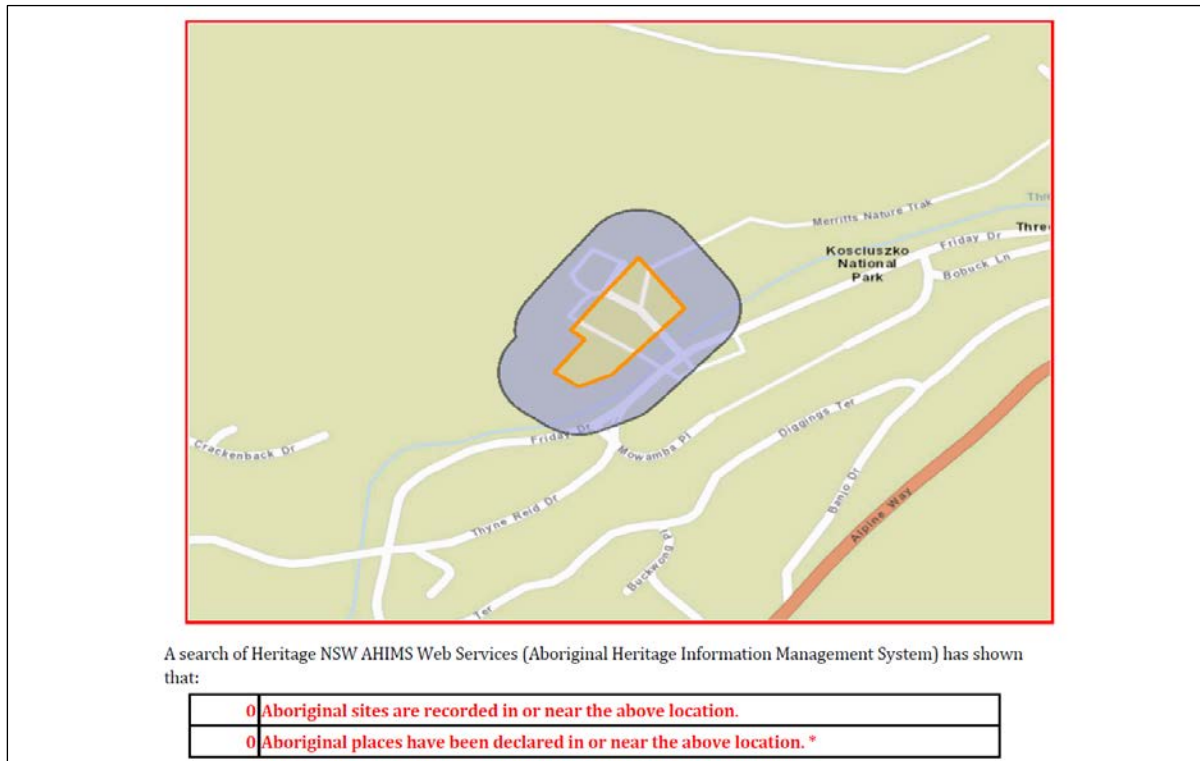




## 2.2 ABORIGINAL HERITAGE

An AHIMS search conducted for Lot 862 DP1128686 on the 03/06/2022 found zero sites or places recorded or declared at the subject allotment. There have been no known Aboriginal artefacts found during previous construction of the existing building and surrounds.

**FIGURE 9 AHIMS SEARCH RESULT LOT 862 DP1128686**



## 2.3 BIODIVERSITY

Lot 862 DP1128686 does contain Biodiversity Values Map (BVM) areas, as demonstrated in Figure 10 below. The Valley Terminal building is not located within the BVM areas, and the proposed repair works will be internal works only. Hence, the proposed works will not be occurring in BVM areas and will not trigger entry into the Biodiversity Offset Scheme (BOS).

**FIGURE 10 BIODIVERSITY VALUES MAP**



## 2.4 RIPARIAN LANDS AND WATERCOURSES

Lot 862 DP1128686 contains areas identified on the Riparian Lands and Watercourses (RLW) map, as indicated in Figure 11 below. A small area of the south-western corner of the Valley Terminal Building is located within the RLW map. The closest building edge is located approximately 40m to the Thredbo River. The proposed internal repair works will be internal works only, and will not cause ground disturbance, nor will they negatively impact upon the Thredbo River.

**FIGURE 11 RIPARIAN LANDS AND WATERCOURSES MAP**



## 2.5 SITE CONTOURS

Landfall is generally consistent across the site, there is a significant upslope to the North-west, and a consistent downslope towards Thredbo River. The slope continues upward to the South-east from Friday Drive through to the Alpine Way.

**FIGURE 12 CONTOUR PLAN**



No excavation works or ground disturbance is proposed as part of this application and therefore there will be no geotechnical issues or impacts associated with the project, and hence, a geotechnical report will not be required.



## 2.6 COLOUR SCHEDULE

No changes are proposed to the external surfaces of the building.

## 2.7 BUILT FORM AND CHARACTER OF ADJOINING DEVELOPMENT

The Valley Terminal building is situated in the heart of Thredbo Village and has played a key role in establishing Thredbo as a popular ski tourist destination.

Surrounding developments are similar in scale and design, offering architectural features including granite stone, loft gable rooflines, feature articulation in structural elements and painted timber cladding. The surrounding buildings are of various architectural vernacular styles - typically built 40 years or more ago with a growing number of new modern style buildings.

There will be no change to the current view sharing.

## 2.8 SITE PHOTOS











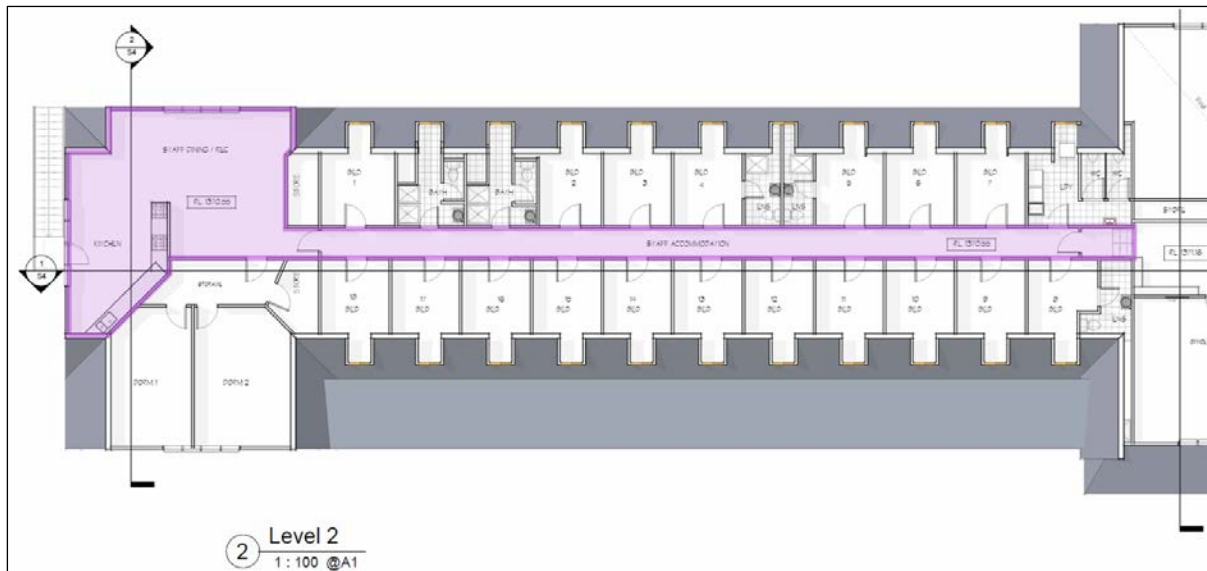


### 3.0 PROJECT

The project for which this Statement applies is for internal repair and maintenance to Thredbo Valley Terminal building. Thredbo Valley Terminal building is located at Lot 862 DP1128686, Friday Drive, Thredbo within the Kosciuszko National Park.

### 3.1 COMMON AREA

**FIGURE 13 STAFF ACCOMMODATION COMMON AREAS**

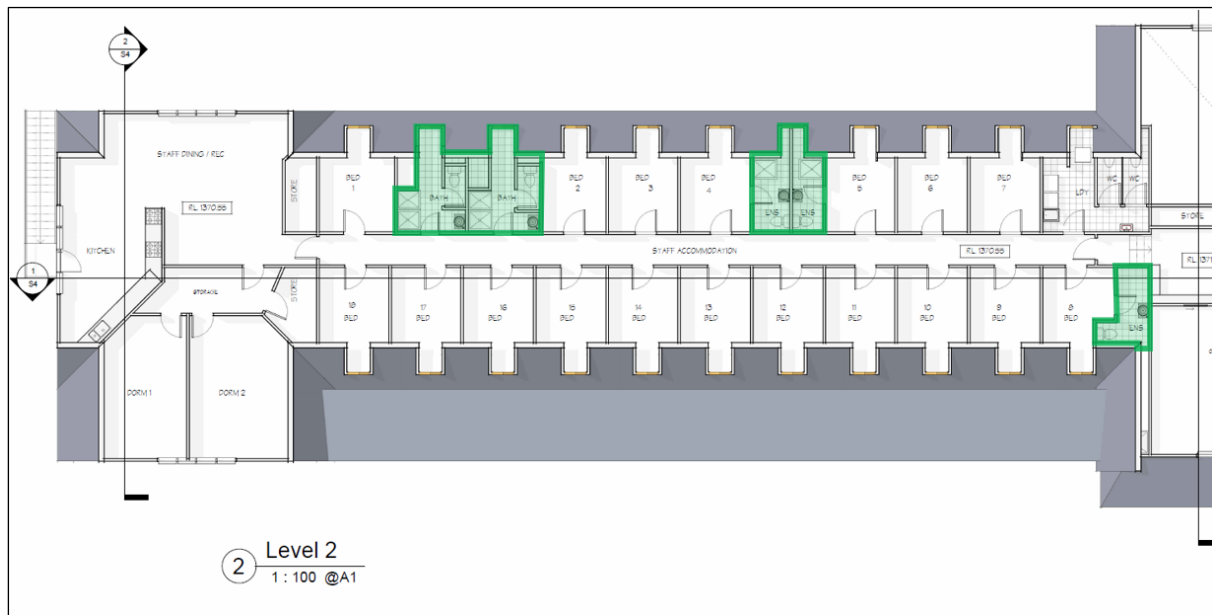


This application is seeking approval for the following works within the staff common areas (for the areas highlighted in purple in Figure 13):

- Removal of existing flooring (carpet and linoleum) to be replaced with a like-for-like flooring product.
- Removal of existing doors and jambs (not fire-rated) to be replaced with like-for-like new fire-rated doors and jambs.
- Removal of all lights to be replaced with upgraded LED lights.
- Removal of all fans, power points and switches to be replaced with like-for-like and upgraded where possible.
- Removal of all kitchen cabinets, sinks and ovens to be replaced with like-for-like upgrades.
- Removal of all asbestos containing walls, to be replaced with Gyprock Fyrcheck MR.
- Removal of all gyprock walls (walls and ceiling) to be replaced with Gyprock Fyrcheck MR.
- Remove all plumbing, to be replaced with a like-for-like upgrade (with isolation valves).
- Remove fire-sprinkler system to be replaced with an upgraded, modern system.

### 3.2 BATHROOMS

**FIGURE 14 STAFF ACCOMMODATION BATHROOMS**



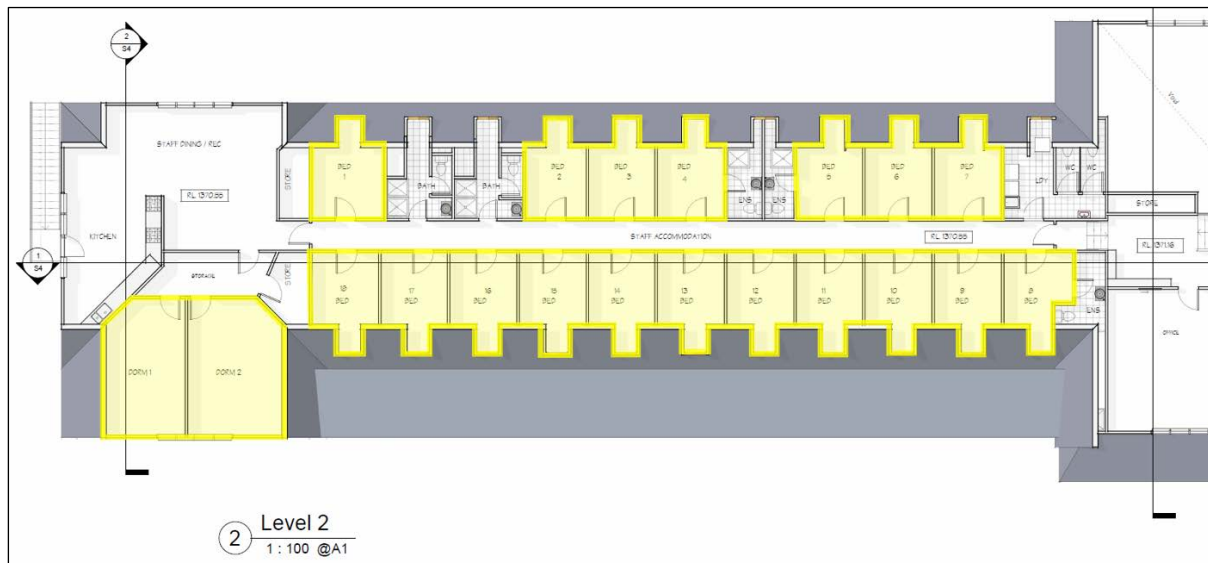
This application is seeking approval for the following works within the staff accommodation bathrooms (for the areas highlighted in green in Figure 14):

- Removal of existing lights, fans, power points and switches to be replaced with like-for-like upgrades (all replacement lights to be LED)
- Removal of toilets, sinks, vanities, shower heads and taps to be replaced with like-for-like upgraded finishes.
- The waterproofing in the bathrooms has failed which is causing water to leak from the wet areas to surrounding walls and the floor below. In order to repair this, all tiles will need to be removed. All wet area walls contain asbestos and removing wall tiles will damage asbestos wall sheets. A solution to this is to remove the wall sheets with the tiles attached to limit disturbance of asbestos containing materials. The walls will then be replaced with Gyprock Fyrcheck MR sheeting, with new waterproofing and like-for-like tiles. The water damaged subfloor will also be removed and replaced with Hardie Panel compressed sheets.
- Removal of all asbestos containing walls, to be replaced with Gyprock Fyrcheck MR.
- Removal of all gyprock walls (walls and ceiling) to be replaced with Gyprock Fyrcheck MR.
- Remove all plumbing, to be replaced with a like-for-like upgrade (with isolation valves).
- Remove fire-sprinkler system to be replaced with an upgraded, modern system.



### 3.3 BEDROOMS

**FIGURE 15 STAFF ACCOMMODATION BEDROOMS**

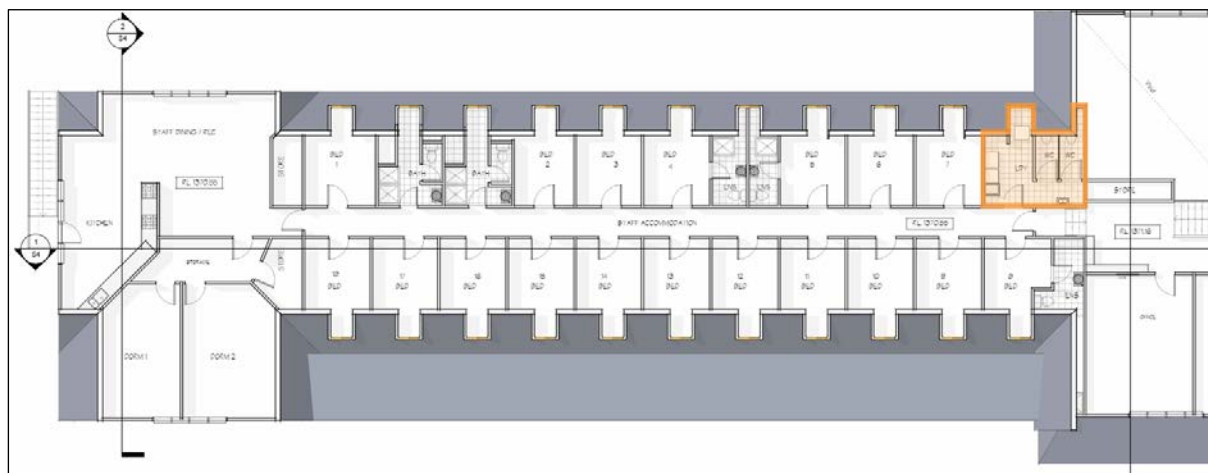


This application is seeking approval for the following works within the staff accommodation bedrooms (for the areas highlighted in yellow in Figure 15):

- Removal of existing flooring (carpet) to be replaced with a like-for-like flooring product.
- Removal of existing doors and jambs (not fire-rated) to be replaced with like-for-like new fire-rated doors and jambs.
- Removal of existing lights, fans, power points and switches to be replaced with like-for-like upgrades (all replacement lights to be LED)
- Removal of all asbestos containing walls, to be replaced with Gyprock Fyrcheck MR.
- Removal of all gyprock walls (walls and ceiling) to be replaced with Gyprock Fyrcheck MR.
- Remove fire-sprinkler system to be replaced with an upgraded, modern system.

### 3.4 LAUNDRY

**FIGURE 16 STAFF ACCOMMODATION LAUNDRY**



This application is seeking approval for the following works within the staff accommodation laundry (for the areas highlighted in orange in Figure 16):

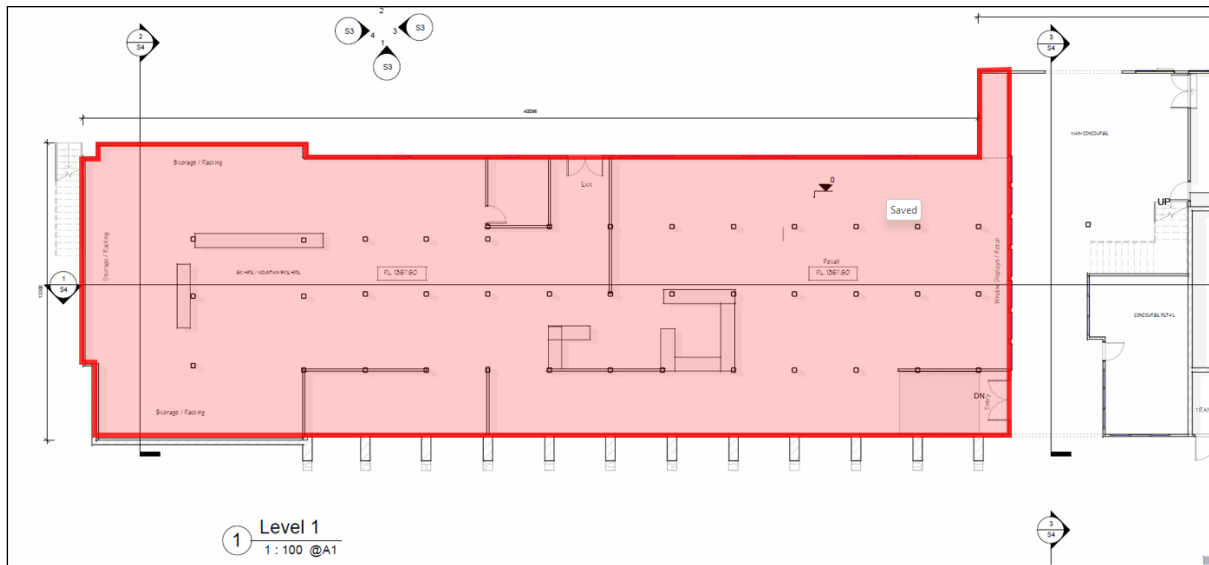
- Removal of existing doors and jambs (not fire-rated) to be replaced with like-for-like new

fire-rated doors and jambs.

- Removal of existing lights, fans, power points and switches to be replaced with like-for-like upgrades (all replacement lights to be LED)
- Removal of all asbestos containing walls, to be replaced with Gyprock Fyrcheck MR.
- Removal of all gyprock walls (walls and ceiling) to be replaced with Gyprock Fyrcheck MR.
- Remove all plumbing, to be replaced with a like-for-like upgrade (with isolation valves).
- Remove fire-sprinkler system to be replaced with an upgraded, modern system.

### 3.5 RETAIL SHOP

**FIGURE 17 VALLEY TERMINAL RETAIL**

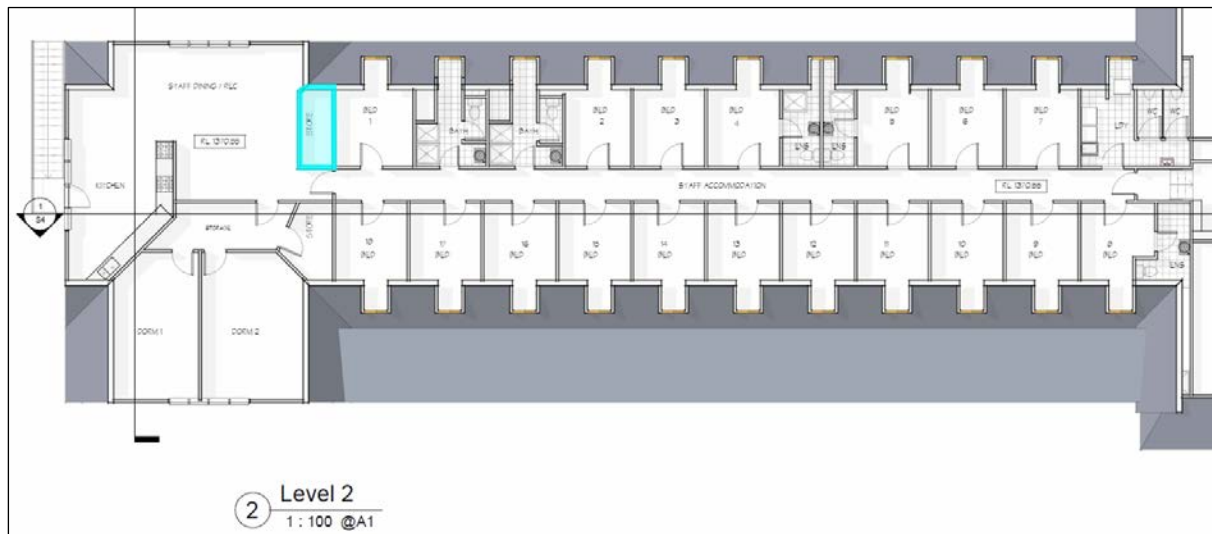


The failed waterproofing in the staff accommodation bathrooms (on the upper level) has caused water damage to the subfloor between the ground floor (retail) and the upper floor, as well as damage to the ceiling and wall linings. For this reason, this application is seeking approval for the repair/replacement of the following (to occur in areas highlighted in red in Figure 17):

- Removal of all asbestos containing walls, to be replaced with Gyprock Fyrcheck MR.
- Removal of all gyprock walls (walls and ceiling) to be replaced with Gyprock Fyrcheck MR.
- Removal of all electrical, the distribution board is very old, hence it is no longer possible to order/find replacement parts. For this reason, all electrical is to be upgraded to current specifications/requirements.

### 3.6 STAFF STORAGE ROOM

**FIGURE 18 STAFF ACCOMMODATION STORAGE ROOM**



Due to the age of the building, the electrical lines and distribution board are largely original. Unfortunately due to its age, it is now no longer possible to order/find replacements parts. For this reason, the electrical distribution board and associated wiring/fittings will need to be removed and replaced with an upgraded system.



## 4.0 GENERAL INFORMATION

### PROJECT DESCRIPTION

The project for which this Statement applies is for internal repair and maintenance work to Valley Terminal Building.

### SITE SUITABILITY

The site is suitable for the proposed development.

<ul style="list-style-type: none"> <li>site constraints such as flooding, slope, geotechnical hazards, bushfire, and any other risks.</li> </ul>	<p>The subject site is identified as bushfire prone land; however, the proposed works are internal works only, and will not alter any external building surfaces. Hence, a Bushfire Hazard Assessment Report (BHAR) is not required as part of this assessment.</p> <p>The subject site is not identified as flood prone land.</p> <p>The proposed works are for internal repair and replacement works only and will not result in any ground disturbance, and the works will not alter the load bearing capacity of the building. Hence, a geotechnical report will not be required as part of this application.</p>
<ul style="list-style-type: none"> <li>effects on the local environment, landscape, streetscape, appearance, or scenic quality of the locality.</li> </ul>	<p>There are no proposed adverse effects of the local environment, landscape, streetscape, appearance, or scenic quality of the locality as the proposed development is for internal repair and maintenance works only.</p>
<ul style="list-style-type: none"> <li>biological and ecological impacts including the impacts on fauna and flora.</li> </ul>	<p>The subject lot does contain areas of High Biodiversity Values. However, the proposed works are internal works only. Hence, the repair and replacement works will not negatively impact upon the BVM areas. No ground disturbance will be occurring as part of this proposal.</p> <p>Caution will be taken in handling and disposing of any waste materials associated with the proposed works, to limit any potential damage or contamination of these areas.</p>
<ul style="list-style-type: none"> <li>impacts on existing and future amenity of the locality.</li> </ul>	<p>The proposed works are deemed to be of positive influence on both the existing building as well as Thredbo Village, by offering a higher level of staff accommodation which will enhance the tourism service industry in the area.</p>
<ul style="list-style-type: none"> <li>the age and condition of any structures or buildings.</li> </ul>	<p>Thredbo Valley Terminal was constructed in the 1960's. Since construction, the building has deteriorated in the highlighted areas, hence the need for repairs and upgrades.</p>

### PRESENT AND PREVIOUS USES

*"The Valley Terminal building was a central development for the early head lessees, being the public area where all skiers passed through to gain access to the ski slopes"* (OzArk, Historic Heritage Assessment, 2021). Today, the VT building offers retail spaces for the public, and a range of office spaces for Thredbo staff, alongside staff accommodation.

<b>OPERATIONAL DETAILS</b>
The building includes retail space, a café, a medical practice, office spaces as well as staff accommodation. There are no proposed changes to the current operational details for the VT building.
<b>CHANGE OF USE OF A BUILDING (WHERE THERE IS NO BUILDING WORK)</b>
Not applicable.
<b>BUILDING CLASSIFICATION AND BUILDING CODE OF AUSTRALIA (BCA)</b>
The VT building includes class 3, 5 and 6 building classes.
<b>SNOW DEPOSITION</b>
The roof design of the VT building is appropriately sloped, and directs accumulated snow to fall away from entrances, establishing safe entry and exit into and out of the VT building.
<b>GEOTECHNICAL ENGINEERING SUMMARY</b>
Not applicable, no ground disturbance will occur as part of this application.
<b>STRUCTURAL ENGINEERING DETAILS</b>
No structural work is proposed as part of this application.
<b>SOCIAL AND ECONOMIC IMPACT</b>
<p>The proposed maintenance works are expected to have positive social and economic impacts. The VT building is recognised as one of the original buildings within Thredbo Village that helped pave the way for Thredbo to become what it is today. The maintenance of the building will improve the longevity of the building and ensure the continuation and operation of businesses; hence the works will contribute to the continued economic function of Thredbo Village. The significance of the building will also be maintained; hence it is crucial that these works are undertaken urgently to prevent further deterioration and damage to the building.</p> <p>Additionally, the maintenance of the VT building will secure the long-term resilience of the resort and will protect the seasonal jobs offered annually each ski season.</p>
<b>ACCESS AND TRAFFIC</b>
Thredbo Village is accessible by car on a year-round basis. The proposed development does not include the addition of any bed licences or vehicle parking areas and is not expected to generate additional traffic to the resort. For this reason, the proposed development will have no impact on the existing access and traffic to Thredbo Village.
<b>PRIVACY, VIEWS AND OVERSHADOWING</b>
There are no proposed changes to the size, layout and/or footprint of the VT building, and the maintenance work will be internal works only. Hence, the proposed works are not anticipated to have any negative impacts to privacy, views and overshadowing.
<b>AIR AND NOISE</b>
<p><b>AIR</b></p> <p>Works involving dust dispersion will use water spray to help keep material damp and dust down. Covers will be placed over waste storage areas and piles of excavated materials to prevent dust dispersion. When transporting materials that cause dust they will be dampened and covered before moving.</p> <p><b>NOISE</b></p> <p>Noise on a construction site can become a form of pollution to the local environment through the use of plant, machinery and tools. For protection of employees and visitors to the site they are issued with PPE including ear protection.</p> <p>To reduce noise pollution from site the following procedures will be followed:</p> <ul style="list-style-type: none"> <li>• All plant, machinery and tools will be maintained in good working order at all times.</li> <li>• Work involving noisy tools or machinery to be used inside the building structure when possible.</li> </ul>

<ul style="list-style-type: none"> <li>• Strict hours of operation for each site will be implemented to reduce noise pollution to the surrounding areas.</li> <li>• In the instance of receiving a complaint in regard to noise levels immediate rectification will occur as far as practical.</li> </ul>
<b>SOIL, WATER AND WASTEWATER MANAGEMENT</b>
The proposed works do not involve any ground disturbance, and there are no proposed changes to existing water and wastewater management.
<b>HERITAGE</b>
<p>The Valley Terminal Building is identified as a Heritage Item under <i>Schedule 3 Heritage Items – Chapter 4 of the Precincts Regional SEPP 2021</i>. The proposed works will have an acceptable heritage impact, given that the significance of the VT building and its ability to contribute to the Thredbo Village will be retained. The proposed repair and maintenance works will have no adverse impact on the heritage significance of the building, in fact the works will improve the condition, and therefore longevity of the building to ensure the continuation of its operation and significance.</p> <p>The removal of some fabric is deemed as acceptable, given that the proposed maintenance work will sympathetically replace existing deteriorated fabric, with a new like-for-like product to match the existing in terms of profile, material, colour and finishes.</p>
<b>ABORIGINAL CULTURAL HERITAGE</b>
An AHIMS search for Lot 862 DP1128686 found zero Aboriginal sites and places recorded in the lot. There have been no known Aboriginal artefacts found during previous construction of the VT building and surrounding areas. The proposed works will be internal works only and do not include any ground disturbance, hence the likelihood of finding Aboriginal artefacts as part of the proposed works is highly unlikely.
<b>VEGETATION REMOVAL</b>
Not applicable, the proposed works are internal works only, and do not require the removal of any vegetation.
<b>ENERGY</b>
If required, a Section J assessment can be undertaken during construction certificate.
<b>WASTE</b>
<p>The proposed development is not expected to increase visitation numbers to the resort. As a result, there will be no need to increase the capacity of existing waste disposal facilities due to the proposed development.</p> <p>Waste generated from the removal of the deteriorated materials and the general construction waste and other waste associated with the maintenance work to the VT building will be disposed of in the skip bin and deposited in the Thredbo waste facility.</p>
<b>DEMOLITION</b>
Waste generated from the proposed repair and maintenance works will be disposed of in the skip bin and deposited in the nearest council waste facility.
<b>TERMITE PROTECTION</b>
To be conditioned as part of the Development Consent.
<b>SCHEDULE OF WORKS</b>
Construction will be during the summer season from October to May with no works proposed to be conducted during the winter season June to September. The start date for construction is not yet set and the year will be determined after development consent has been issued.



## 5.0 PERMISSIBILITY, LEGISLATION AND REGIONAL PLAN

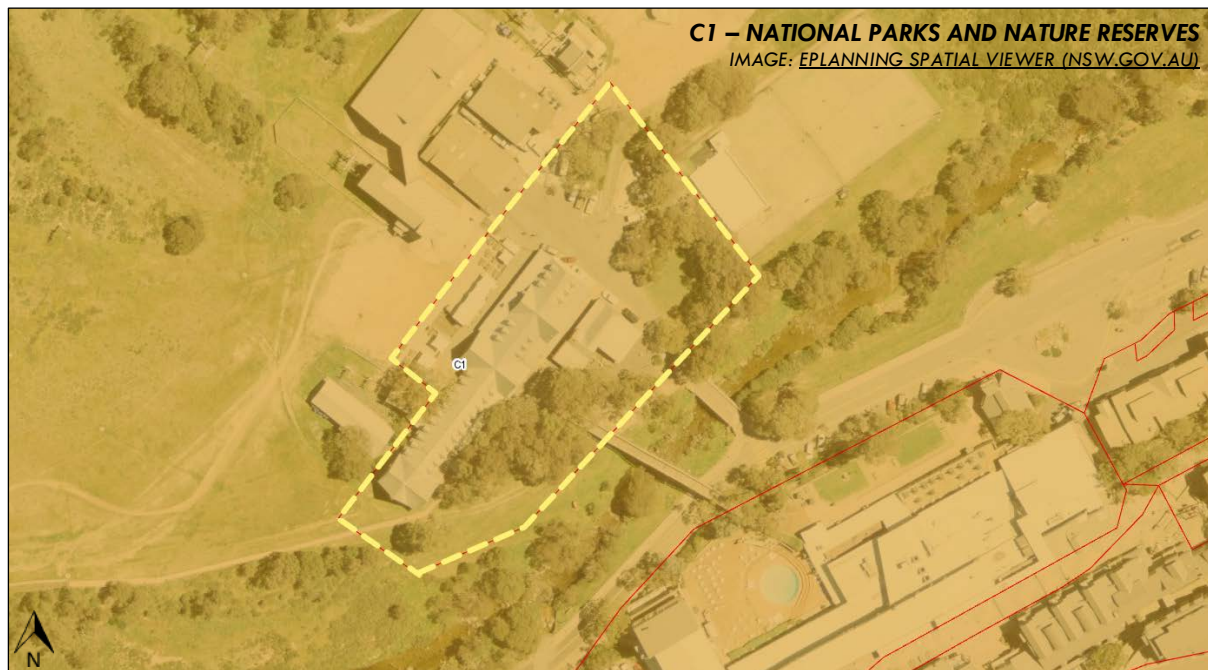
### 5.1 SOUTH EAST AND TABLELAND REGIONAL PLAN 2036

The proposed upgrades to Thredbo Village will improve the longevity of the VT building by maintaining and replacing external materials that are deteriorated. This is consistent with the purpose of the Regional Plan, which offers a framework to provide infrastructure and development to help strengthen the long-term resilience of the South-East and Tableland region. The regional Plan seeks to acknowledge the unique environmental and cultural significance of Kosciuszko National Park, whilst also enhancing visitor experience, which this application has taken into consideration.

### 5.2 STATE ENVIRONMENTAL PLANNING POLICY (PRECINCTS – REGIONAL) 2021

The proposal is for internal repair and maintenance works to the existing VT building, which is permissible under Chapter 4 of the State Environmental Planning Policy (Precincts – Regional) 2021.

**FIGURE 14 LAND ZONING MAP**



#### **THREDBO ALPINE RESORT**

##### **PERMITTED WITHOUT CONSENT**

*Nil.*

##### **PERMITTED WITH CONSENT**

*Advertisements; Building identification signs; Business identification signs; Car parking; Commercial premises (other than brothels and workshops); Community facilities; Conference facilities; Educational establishments; Emergency services facilities; Entertainment facilities; Fences; Food outlets; Health profession consulting rooms; Helipads; Infrastructure facilities; Lifting facilities; Management trails; Medical centres; Places of public worship; Public utility undertakings; Recreation facilities; Recreation infrastructure; Shops; Ski slope huts; Ski slopes; Snow-making infrastructure; Stream flow monitoring stations; Telecommunications facilities; Tourist accommodation; Transport facilities; Vehicle repair stations; Weather stations.*

##### **PROHIBITED**

*Any development not otherwise specified in item 1 or 2.*

**5.2.1 AIMS & OBJECTIVES OF CHAPTER (PART 4.1 PRECINCTS – REGIONAL SEPP)**

(1) The aim of this Chapter is to protect and enhance the Alpine Region by ensuring development is managed with regard to the principles of ecologically sustainable development, including the conservation and restoration of ecological processes, natural systems and biodiversity.	<p>Noted. The proposed works are consistent with the aims and objectives of the policy as the development will improve the condition and therefore longevity of the building.</p> <p>The proposal will create positive social and economic impacts and works towards securing the long-term resilience of the alpine resort.</p>
<p>(2) The objectives of this Chapter are as follows—</p> <p>(a) to encourage the carrying out of a range of development to support sustainable tourism in the Alpine Region all year round, if the development does not result in adverse environmental, social or economic impacts on the natural or cultural environment of the Alpine Region, including cumulative impacts on the environment from development and resource use,</p> <p>(b) to establish planning controls that—</p> <p>(i) contribute to and facilitate the carrying out of ecologically sustainable development in the Alpine Region, and</p> <p>(ii) recognise the Alpine Region's significant contribution to recreation and the tourism economy in the State,</p> <p>(c) to minimise the risk to the community of exposure to environmental hazards, particularly geotechnical hazards, bush fires and flooding, by—</p> <p>(i) generally requiring development consent on land in the Alpine Region, and</p> <p>(ii) establishing planning controls for buildings to ensure the safety of persons using the buildings if there is a fire.</p>	<p>The proposed works are expected to have minimal environmental impact, based upon the scale of the works, and the maintenance works will be occurring internally to an existing building. With the implementation of appropriate environmental controls during construction, the potential impacts from the works on the natural environment are negligible.</p> <p>The proposed development has been designed so as to not disturb or detriment the surrounding natural environment.</p>

**5.2.2 OTHER DEVELOPMENT CONTROLS (PART 4.4 PRECINCTS – REGIONAL SEPP)**

4.21 Heritage Conservation	
(1) The objective of this section is to conserve—	Noted.
<p>(a) the environmental heritage of the Alpine Region, and</p> <p>(b) the heritage significance of heritage items, including associated fabric, settings and views, and</p> <p>(c) Aboriginal heritage items and Aboriginal places.</p>	
(2) Development consent is required for the following in the Alpine Region—	The Valley Terminal Building is identified as a Heritage Item under Schedule 3 Heritage Items – Chapter 4 of the Precincts Regional SEPP 2021. The proposed works will have an acceptable heritage impact, given that the significance of the VT building and its ability to contribute to the Thredbo Village will be retained. The proposed internal maintenance
<p>(a) demolishing or moving a heritage item,</p> <p>(b) altering a heritage item, including by doing the following to a heritage item that is a building—</p>	

<ul style="list-style-type: none"> <li>(i) making changes to the detail, fabric, finish or appearance of the building's exterior,</li> <li>(ii) making structural changes to the building's interior,</li> <li>(c) disturbing or excavating land that is, or contains, an Aboriginal heritage item,</li> <li>(d) erecting a building on land that is, or contains, a heritage item,</li> <li>(e) subdividing land that is, or contains, a heritage item.</li> </ul>	<p>works will have no adverse impact on the heritage significance of the building, in fact the works will improve the condition, and therefore longevity of the building to ensure the continuation of its operation and significance.</p> <p>The proposed internal maintenance work will sympathetically replace existing deteriorated finishes.</p>
<p>(3) Development consent is not required under this section for the following development—</p> <ul style="list-style-type: none"> <li>(a) development that involves only the removal of a tree or other vegetation that the consent authority is satisfied is a risk to human life or property,</li> <li>(b) exempt development,</li> <li>(c) development that does not require development consent under section 4.14.</li> </ul>	Noted.
<p>(4) The consent authority may, before work is carried out, give written notice to an applicant for development consent that development consent is not required under this section if the consent authority is satisfied the development—</p> <ul style="list-style-type: none"> <li>(a) is of a minor nature or is for the maintenance of the heritage item, and</li> <li>(b) the development will not adversely impact the heritage significance of the heritage item.</li> </ul>	Noted.
<p>(5) In deciding whether to grant development consent for development under this section, the consent authority—</p> <ul style="list-style-type: none"> <li>(a) must consider the effect of the development on— <ul style="list-style-type: none"> <li>(i) the heritage significance of the heritage item, and</li> <li>(ii) Aboriginal objects known or reasonably likely to be located on the land,</li> </ul> </li> <li>(b) may require the submission of a heritage conservation management plan, and</li> <li>(c) for development on land that is, contains or is near a heritage item— may require the preparation of a heritage impact statement.</li> </ul>	<p>The proposed works will have an acceptable heritage impact, given that the significance of the VT building and its ability to contribute to the Thredbo Village will be retained. The proposed maintenance works will have no adverse impact on the heritage significance of the building.</p>
<p>(6) Development consent must not be granted to development on land that is, or contains, an Aboriginal heritage item, and that requires development consent under this section, unless the consent authority has—</p> <ul style="list-style-type: none"> <li>(a) given written or other appropriate notice of the development to the local Aboriginal communities, and</li> <li>(b) considered responses received from the communities within 28 days after the notice is given.</li> </ul>	Not applicable.
4.22 Conservation incentives	
<p>(1) This section applies to development in the Alpine Region that—</p>	Noted.



<p>(a) involves a building that is a heritage item, or</p> <p>(b) is on land on which a building that is a heritage item is located.</p>	
<p>(2) Development consent may be granted to development to which this section applies, even if the development would otherwise be prohibited under this Chapter, if the consent authority is satisfied of the following—</p> <p>(a) granting the development consent will facilitate the conservation of the heritage item,</p> <p>(b) the development will be in accordance with a heritage conservation management plan that has been approved by the consent authority,</p> <p>(c) the development consent will require the carrying out of all necessary conservation work identified in the heritage conservation management plan,</p> <p>(d) the development will not adversely affect the heritage significance of the heritage item, including its setting,</p> <p>(e) the development will not have a significant adverse impact on the amenity of the surrounding area.</p>	<p>The proposed works will facilitate the conservation of the VT building as an identified Heritage Item. There will be no adverse affects on the heritage significance of the building as all internal works will be upgraded with 'like' materials.</p>

### 5.2.3 DEVELOPMENT ASSESSMENT & CONSENT (PART 4.5 PRECINCTS – REGIONAL SEPP)

<b>4.26 Master plans</b>	
(1) The Minister must prepare and approve a master plan that applies to the Alpine Region.	Noted.
<p>(2) The master plan must contain the following information—</p> <p>(a) the strategic vision and general objectives for the Alpine Region,</p> <p>(b) a map showing existing and proposed types of development,</p> <p>(c) the performance criteria for development,</p> <p>(d) information about heritage items or places of heritage significance,</p> <p>(e) limitations on development on certain land, including environmentally sensitive areas, land prone to flooding and cultural heritage.</p>	Noted.
(3) The master plan may also contain proposals for infrastructure facilities, public utility undertakings, roads and transport.	Noted.
(4) The master plan must be consistent with this Chapter.	Noted.
(5) The Minister may amend or replace a master plan.	Noted.

(6) A draft master plan must be published on the NSW planning portal for at least 28 days before it is approved by the Minister.	Noted
(7) A master plan approved by the Minister must be published on the NSW planning portal and takes effect on the day it is published.	Noted
<b>4.27 Consultation with National Parks and Wildlife Service</b>	
(1) Development consent must not be granted to development in the Alpine Region unless the consent authority has— (a) consulted with the National Parks and Wildlife Service, and (b) considered submissions received from the National Parks and Wildlife Service within the relevant period.	Noted
(2) In this section— relevant period means— (a) 28 days after notice of the development application is given to the National Parks and Wildlife Service, or (b) another period determined by the Planning Secretary.	Noted
<b>4.28 Consideration of master plans and other documents</b>	
(1) In deciding whether to grant development consent to development in the Alpine Region, the consent authority must consider the following— (a) the aim and objectives of this Chapter set out in section 4.1, (b) a draft development control plan that is intended to apply to the land and has been published on the NSW planning portal, (c) a conservation agreement under the Environment Protection and Biodiversity Conservation Act 1999 of the Commonwealth that applies to the land, (d) the Geotechnical Policy —Kosciuszko Alpine Resorts published by the Department in November 2003, (e) for development in the Perisher Range Alpine Resort— (i) the Perisher Range Resorts Master Plan, published by the National Parks and Wildlife Service in November 2001, and (ii) the Perisher Blue Ski Resort Ski Slope Master Plan adopted by the National Parks and Wildlife Service in May 2002.	Complies.  N/A  N/A Internal works only  N/A not earthworks proposed. Internal works only  N/A the development is not within Perisher Range.
(2) In deciding whether to grant development consent to development in the Alpine Region, the consent authority must consider— (a) a master plan approved by the Minister under section 4.26 that applies to the land, or (b) if a master plan has not been approved—a draft master plan prepared under section 4.26 that is intended to apply	Complies. See Chapter 5.3 of this report.

to the land and has been published on the NSW planning portal.	
<b>4.29 Consideration of environmental, geotechnical and other matters</b>	
<p>(1) In deciding whether to grant development consent to development in the Alpine Region, the consent authority must consider the following—</p> <ul style="list-style-type: none"> <li>(a) measures proposed to address geotechnical issues relating to the development,</li> <li>(b) the extent to which the development will achieve an appropriate balance between— <ul style="list-style-type: none"> <li>(i) the conservation of the natural environment, and</li> <li>(ii) taking measures to mitigate environmental hazards, including geotechnical hazards, bush fires and flooding,</li> </ul> </li> <li>(c) the visual impact of the proposed development, particularly when viewed from the land identified as the Main Range Management Unit in the Kosciuszko National Park Plan of Management,</li> <li>(d) the cumulative impacts of development and resource use on the environment of the Alpine Subregion in which the development is carried out,</li> <li>(e) the capacity of existing infrastructure and services for transport to and within the Alpine Region to deal with additional usage generated by the development, including in peak periods,</li> <li>(f) the capacity of existing waste or resource management facilities to deal with additional waste generated by the development, including in peak periods.</li> </ul>	<p>N/A. Internal works only</p> <p>N/A. Internal works only</p> <p>The proposed works will not change the scale, design or footprint of the existing VT building. Hence, they will have no impacts to the visual and scenic qualities of the resort.</p> <p>The proposed internal works will have no impact.</p> <p>The internal works will have no change on the existing use of the building and therefore will have no increase to the capacity of the supporting infrastructure.</p> <p>The proposal will not generate additional waste as no change in use is proposed.</p>
(2) For development involving earthworks or stormwater draining works, the consent authority must also consider measures to mitigate adverse impacts associated with the works.	N/A There is no earthworks or exaction work proposed. There are no proposed changes to the existing stormwater drainage measures currently used for the VT building.
<p>(3) For development the consent authority considers will significantly alter the character of an Alpine Subregion, the consent authority must also consider—</p> <ul style="list-style-type: none"> <li>(a) the existing character of the site and immediate surroundings, and</li> <li>(b) how the development will relate to the Alpine Subregion.</li> </ul>	The proposed works are all internal and will not change the scale, design or footprint of the existing VT building and how the building relates to the immediate surroundings and the Alpine Subregion.
<b>4.30 Kosciuszko National Park Plan of Management</b>	
(1) Development consent may be granted to development in the Alpine Region even if the application has not established that the development is consistent with the Kosciuszko National Park Plan of Management.	The development is consistent with the Kosciuszko National Park Plan of Management.
(2) This section does not prevent the consent authority from refusing to grant consent to	Noted.



<p>development on the basis that the development is not consistent with the Kosciuszko National Park Plan of Management.</p> <p>Note—</p> <p>Under the National Parks and Wildlife Act 1974, section 81(4), operations on land to which a plan of management under that Act applies may be undertaken only if they are undertaken in accordance with the plan of management, despite another Act or an instrument made under an Act.</p>	
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### 5.3 SNOWY MOUNTAINS SPECIAL ACTIVATION PRECINCT MASTER PLAN JULY 2022

#### 5.3.1 CHAPTER 10 ALPINE PRECINCT PROVISIONS

<b>Land Use (10.1) Performance Criteria</b>	
A. Development is to be permissible and consistent with the Master Plan, Precincts—Regional SEPP, Alpine Development Control Plan, Kosciuszko National Park Plan of Management, and the National Parks and Wildlife Act.	Complies. Alpine DCP is not yet in effect.
B. In considering the suitability of the development, the consent authority must be satisfied that the development meets the performance criteria and development controls in this Master Plan and in the Alpine Development Control Plan.	Complies with Master Plan. Alpine DCP is not yet in effect.
<i>Appropriate locations for alpine development</i>	
C. Development consent can only be issued for development in the Alpine Precinct where: <ul style="list-style-type: none"> <li>i. the uses will support the diversification of the Alpine Precinct's tourism offering and year-round economic viability.</li> <li>ii. the uses will not compromise the environmental, heritage and cultural values of the Alpine Precinct.</li> <li>iii. the uses will not exceed the established carrying capacity of the Alpine Precinct.</li> </ul>	Complies. The proposed internal repair work will not change the use of the VT building and add to the use of the Alpine Precinct.
D. The location of future development should align with the relevant structure plan and be focused on land marked 'Development area'. Where development is proposed on land outside these areas, additional technical investigation may be required.	N/A. Alterations to an existing building.
E. Development for new or upgraded accommodation will meet the indicative sub-precinct yields and visitor thresholds set out in the Kosciuszko National Park Plan of Management and leasing arrangements. Refer also to Chapter 14 of this Master Plan	N/A
<b>Alpine Resorts (10.2) Performance Criteria</b>	
A. Development should contribute to visitor attraction and village experience through: <ul style="list-style-type: none"> <li>i. the prioritisation of infill development.</li> <li>ii. improvements to pedestrian and active transport connections.</li> <li>iii. creation and implementation of active</li> </ul>	N/A Existing.

street frontages.	
B. Development should integrate public transport opportunities and should create gateways and nodes to create a sense of place and community in Alpine Resort sub-precincts.	Existing
C. Development should provide a range of tourist accommodation offerings and seasonal worker accommodation.	Existing staff accommodation
D. Development should be designed to reduce on-site power consumption and improve environmental performance	Repairs to external linings will improve thermal efficiency of the building.
E. Development should be designed to contribute to the alpine character of the Alpine Resorts and reflect the alpine landscape and natural environment.	Complies. The proposed maintenance works will retain the original alpine character of the Thredbo Village
<b>Alpine Accommodation (10.3) Performance Criteria</b>	
These provisions are specific to development within the following Alpine Accommodation sub-precincts:• Ski Rider• Sponars Chalet • Thredbo Ranger Station• Creel Bay• Kosciuszko Tourist Park• Island Bend	N/A
<b>Alpine Experience (10.4) Performance Criteria</b>	
A. Public transport or mass transit connections should be integrated into the design of new developments, particularly in Alpine Resort and Alpine Accommodation sub-precincts.	N/A Existing development
B. Transport development must provide safe, reliable and accessible connections into and around the Kosciuszko National Park.	N/A
C. Development should be designed and staged to support and enable the ultimate growth of accommodation and attractions in the Alpine Region.	Complies.
D. Development of new and upgraded shared trails and paths should provide appropriate facilities and amenities.	N/A
E. Development should provide adequate car parking as part of a range of transport solutions (including the provision of accessible parking spaces).	N/A Existing
F. Visitor attractions must be supported by appropriate amenities, facilities and car parking and must minimise its impact to the natural environment.	Complies. Existing
G. Visitor attractions should be designed and staged to support and enable the ultimate growth of attractions in the Alpine Region.	Existing.

### 5.3.2 CHAPTER 11 ENVIRONMENT & SUSTAINABILITY

<b>Biodiversity (11.1) Performance Criteria</b>	
A. All development is to apply the avoid, minimise and offset methodology.	Complies. Internal works only.
B. Development is to avoid threatened ecological communities and threatened species habitat; such vegetation should not be removed. Development may occur in these areas if it is for essential infrastructure	Complies. Internal works only.

C. Development should be focused on colocation and infill to minimise biodiversity impacts	N/A
D. Development should be concentrated in and around already disturbed areas. Where possible, development should provide a buffer between areas of high ecological value and buildings and structures.	Complies. Internal works only.
E. Development should consider the biodiversity impacts of bushfire asset protection zones (APZ) and associated vegetation management.	Complies.
F. Development must offset any impacts to biodiversity through direct management measures within Kosciuszko National Park and should be related to the biodiversity impacted	N/A biodiversity is not impacted.
G. Riparian corridors must be preserved while ensuring consistency with the proposed Flooding and Drainage Strategy for the Precinct.	<p>The subject site does contain areas identified as "Riparian Lands and Watercourses", as the VT building is located to nearby Thredbo River. However, the proposed works are internal works only, hence there are no anticipated negative impacts to nearby Riparian Lands and Watercourses.</p> <p>Waste should be handled carefully and disposed of correctly on site to avoid any contamination of the local waterway.</p>
H. Any revegetation or planting within Kosciuszko National Park should follow the Rehabilitation Guidelines for the Resort Areas of Kosciuszko National Park	As the works are all internal there is no planting or revegetation proposed as part of this application.
<b>Geotechnical (11.2) Performance Criteria</b>	
A. Development must address the requirements of the Geotechnical Policy – Kosciuszko National Park (DPNIR, 2003). This includes: i. development on land covered by the geotechnical maps, under the above policy must ensure the requirements of the policy are met. ii. development on land not covered by the geotechnical maps under the above policy must ensure the requirements of the policy are met and should also use the risk susceptibility mapping to inform the requirements and design of development	The proposed works are for internal repair and replacement works only and will not result in any ground disturbance, and the works will not alter the load bearing capacity of the building. Hence, a geotechnical report will not be required as part of this application.
B. Development must include an assessment of geotechnical risks.	No earthworks are being undertaken.
C. Buildings and structures must be designed to accommodate the specific geotechnical risks identified for the site	N/A Existing
D. Excavations required for new developments must consider the potential to cause widespread slope instability and ensure appropriate mitigation measures are implemented to minimise and manage risk.	N/A
<b>Flood Management Risk (11.3) Performance Criteria</b>	
A. The Flood Planning Level is the 1% AEP plus 500mm freeboard to ensure consistency across the Precinct. Development must generally occur outside the Flood Planning Level unless it can demonstrate that risks can be suitably managed.	N/A



This allows for the maintenance of flood function and to avoid adverse effects on flood behaviour to the detriment of other properties or the environment of the floodplain	
B. Development within the Flood Planning Level should demonstrate that: i. all structures are constructed with flood compatible building components below the 1% AEP flood level plus 500mm freeboard. ii. all structures are designed to withstand the forces of floodwater, debris and buoyancy up to 1% AEP flood plus 500mm freeboard	N/A
C. Development within the Probable Maximum Flood area should demonstrate that: i. all emergency and evacuation infrastructure is to be constructed with flood compatible building components below Probable Maximum Flood level plus 500mm freeboard. ii. all emergency and evacuation infrastructure structures are to be designed to withstand forces of floodwater, debris, and buoyancy up to Probable Maximum Flood plus 500mm freeboard. iii. development must be sited, designed and located to avoid or mitigate the flood risk to people, property and infrastructure such that: <ul style="list-style-type: none"> <li>flood risk is managed through site-specific built form and design.</li> <li>sensitive, vulnerable and critical uses are avoided in the floodplain.</li> </ul>	N/A
D. Development should mitigate the impacts of local overland flooding through the provision of adequate site drainage systems, where possible.	N/A
E. Development must consider and plan for emergency evacuation situations to ensure the safety of all areas within the Probable Maximum Flood extent.	N/A
<b>Water Quality (11.4) Performance Criteria</b>	
A. Maintain or improve the ecological condition of waterbodies and their riparian zones in catchments over the long term	The proposed repair works do not have the potential to negatively impact upon nearby terrestrial and aquatic habitats. The proposed works are internal works only. Hence, the maintenance works ensure the protection of native flora and fauna within and surrounding the nearby Thredbo River.
B. Development in the Alpine Precinct should implement on-site water management and water quality systems through: i. the capture and re-use of water on-site. ii. the treatment of water on-site with any water discharged back into catchments having a neutral or beneficial effect on water quality. iii. incorporating water sensitive urban design principles into the development's-built form and landscaping, where possible.	Existing on-site water management and water quality systems will not be altered as part of this application.
C. The quality of stormwater discharged into receiving catchments must be pre-development quality or better in relation to pH, total suspended solids, total phosphorus, total	Existing stormwater systems will not be altered as part of this application.

nitrogen and gross pollutants. The quality of water should aim to meet the following targets: i. Total Suspended Solids: 85% reduction. ii. Total Phosphorus: 60% reduction. iii. Total Nitrogen: 45% reduction.	
D. The quality of water discharged into receiving catchments should maintain electrical conductivity levels. Water quality should aim to maintain an electrical conductivity below the 30 $\mu\text{S}/\text{cm}$ ANZG 2018 Guideline value for upland rivers of South-East Australia	No change
E. Monitor macroinvertebrates to ensure they are consistently within Band A of the NSW AUSRIVAS model.	N/A
F. Erosion and sediment control should be managed during construction to ensure impacts to waterways are minimised in accordance with Managing Urban Stormwater Soils and Construction, also known as the Blue Book (current edition)	N/A no earthworks are proposed.
G. Discharge of wastewater and/or contaminated stormwater to watercourses or waterways is not permitted unless other specified in an environmental protection licence issued under the Protection of the Environment Operations Act 1997. Development must obtain the appropriate water licenses in accordance with the Water Act 1912 and the Water Management Act 2000 and consider the relevant Water Sharing Plan	Noted.
<b>Bushfire (11.5) Performance Criteria</b>	
A. Development is to: i. minimise perimeters exposed to the bushfire hazard. ii. minimise vegetated corridors that permit the passage of bushfire towards development. iii. provide for the siting of future development away from ridge-tops and steep slopes, within saddles and narrow ridge crests. iv. ensure capacity of existing infrastructure (such as roads and utilities) can accommodate the increase in demand during emergencies as a result of the development.	The subject site is identified as being in bushfire prone land, and hence as outlined in Planning for Bushfire Protection – PBP 2019 (NSW RFS 2019) however, the works are internal works only. The external building surface will not be altered as a result of this application, hence a Bushfire Safety Authority is not required as part of this application.
B. Asset Protection Zones are to be provided and maintained between a bushfire hazard and future development and are designed to address the relevant bushfire attack mechanisms	N/A
C. Adequate access is to be provided from all properties to the wider road network for park users emergency services and to provide access to hazard vegetation to facilitate bushfire mitigation works and fire suppression	Friday Drive is a two-wheel drive, all weather road. The widths and design would allow safe access for firefighters while residents are evacuating an area. The capacity of road surfaces is sufficient to carry fully loaded firefighting vehicles.
D. Development is to minimise levels of radiant heat, localised smoke and ember attack through development design and siting	N/A Internal works only
E. The subdivision of land and location of developments should consider the future uses of land and the inclusion of roads into Asset	N/A

Protection Zones	
<b>Sustainability &amp; Climate Change (11.6) Performance Criteria</b>	
A. Development must be inclusive and sustainable and promote year round use.	Complies.
B. Development should preserve the Precincts landscape, cultural, heritage and biodiversity values by avoiding and minimising impact.	No impact. Internal works only
C. Development should support sustainable and active transport opportunities and integrate open space. Buildings are to express a strong commitment to ESD principles and incorporate passive design, optimal orientation, effective sun shading, cross ventilation and open plan living. This should be evident in the external architectural expression.	Existing
D. Development should comply with applicable sustainability tools and programs for design, construction and operation.	Complies.
E. Consideration must be given to climate responsiveness and resilience. Climate change risks, hazard and opportunities must be considered in the design, construction and operation of development within the Precinct.	Consideration of materials has been made for the project.
F. Operators, lessees and licensees within the Precinct must prepare and maintain an Environmental Management System in accordance with ISO14001:2015 – Environmental management systems and the requirements of the Plan of Management for Kosciuszko National Park	Noted

### 5.3.3 CHAPTER 12 PLACE & LANDSCAPE

<b>Aboriginal Cultural Heritage (12.1) Performance Criteria</b>	
A. Areas of Aboriginal cultural heritage (included as part of the environmentally sensitive areas map) should not be developed. Development may occur in these areas if it is for essential infrastructure and where further Aboriginal cultural heritage assessment will be undertaken to appropriately mitigate and manage any impacts to Aboriginal cultural heritage items, places or areas.	Works are limited to the exterior of the existing building. No ground works are proposed.
B. Aboriginal culturally significant places and sites should be integrated with areas of environmental significance and green space (where appropriate) across the Precinct. This may continue to evolve as greening opportunities across the Precinct are established	Noted.
C. Development in areas where surveys have not been undertaken require further Aboriginal cultural heritage assessment. These assessments must be carried out in accordance with Guide to investigating, assessing and reporting on Aboriginal cultural heritage in NSW (as modified from time to time) prior to any development on this land. These assessments must include a visual survey of the land. Once suitably assessed, any land identified as having	N/A



<p>Aboriginal cultural heritage significance should be included on the Environmentally Sensitive Areas (ESA) map. Development is to be assessed against the mapped zones of archaeological potential as required by the following:</p> <p>i. development within areas identified as 'disturbed land' do not require any further investigation beyond considering the potential for subsurface archaeological deposits. If current disturbances are considered to cover intact archaeological deposits, further investigation should take place that may include test excavation. Should development encounter any unexpected finds during construction, the procedures under the relevant unexpected finds protocol should be followed.</p> <p>ii. works within areas identified as "moderate ACH potential" or 'high ACH potential' should be avoided. Where development will impact these areas, further Aboriginal cultural heritage assessment must be undertaken. This assessment should include a visual inspection, possibly test excavation if warranted, and participation from the Aboriginal community</p>	
D. Development planned on land in which an Aboriginal object is located should be supported by a heritage impact assessment which should be prepared to assess the extent to which a proposed development would harm Aboriginal objects.	N/A
E. If impact to an Aboriginal object is unavoidable, an Aboriginal Heritage Impact Permit (AHIP) under Part 6 of the National Parks and Wildlife Act 1974 would be required.	Noted.
<b>Historic Heritage (12.2) Performance Criteria</b>	
A. Development in areas defined as 'disturbed land' can occur without further historic heritage investigation however must consider neighbouring heritage items and broader heritage values.	Noted.
B. Development on land where a heritage item is situated, that is a heritage item or is on land adjacent to a heritage item must prepare a statement of heritage impact.	N/A internal maintenance work only
<p>C. Development in areas defined as 'high risk' or 'moderate risk' requires further heritage assessment where the development is likely to materially have a major affect on a heritage item or its value. Development is considered to have a materially major affect if it involves:</p> <p>i. the full or partial demolition of a building.</p> <p>ii. major alterations or additions.</p> <p>iii. major adverse impacts, such as the removal of significant fabric, obscuring key views or dominating a heritage item, or the removal of evidence of significant historical associations; and</p> <p>iv. impact to significant archaeological deposits.</p>	N/A
D. Development in areas defined as 'high risk' or 'moderate risk' requires further heritage	N/A

assessment where the development is likely to materially have a minor affect on a heritage item or value. Development is considered to have a minor affect if it involves (but is not limited to): i. repairs or restoration to fabric. ii. installation of fire safety equipment. iii. installation of disabled access. iv. replacement of awnings, balconies, etc. v. installation of signage or fencing. vi. excavation of areas without archaeological potential. vii. erection of temporary structures. viii. installation of safety and security equipment.	
E. Where development is likely to materially have a major effect on a heritage item or value, further heritage assessment is required. This heritage assessment includes: i. a visual inspection to determine the existing heritage values. ii. an archaeological assessment (if appropriate). iii. preparation of a statement of heritage impact.	The proposed repair and maintenance works will have no adverse impact on the heritage value of the building.
F. Where development will have minor effect on a heritage item or value, a heritage assessment may be required. This heritage assessment may include: a visual inspection to determine the existing heritage values. i. an archaeological assessment (if appropriate). ii. use of a previously prepared heritage study if applicable.	The proposed repair and maintenance works will have no adverse impact on the heritage significance of the building.
G. Development that is likely to have a materially major or minor effect on a heritage item or its value must: i. identify the impacts to the heritage values of an item or place. ii. demonstrate the need for the impact and how alternatives to the impact have been considered. iii. demonstrate how the adverse impacts will be minimised or mitigated.	N/A
H. Development adjacent to a heritage item should ensure impacts to the heritage item are minimised, including through the provision of appropriate curtilages. There may be opportunities to reduce the curtilage to some heritage items if it can be demonstrated the development will not have a significant impact on the heritage item or its value.	N/A
I. Heritage items must be used for purposes that are appropriate to their heritage significance, including adaptive re-use where appropriate.	No change in use is proposed.
J. Development is to ensure long-term heritage conservation outcomes are retained or interpreted to reflect the history of heritage items and places.	Internal maintenance will ensure the longevity of the building.
K. Development should through redevelopment or upgrades remove inappropriate or unsympathetic alterations and additions to	Noted. N/A

heritage items and reinstate significant missing details and building elements, where possible.	
<b>Landscape, Character &amp; Open Space (12.3) Performance Criteria</b>	
A. Development should be designed to sensitively integrate into the landscape and should respond appropriately to the topography and climate of the Alpine Precinct	Existing
B. Development should protect, conserve and enhance the Alpine Precinct's natural environment and create a green infrastructure network, where possible.	N/A Existing
C. Landscaping and public open spaces should include plantings of native species found in surrounding plant communities, which aim to achieve the re-establishment of biodiversity in addition to aesthetic appeal and enhancement of the functionality of an area.	Landscaping is existing.
D. Revegetation and new plantings should follow the Rehabilitation guidelines for the Resort Areas of Kosciuszko National Park	No revegetation is proposed.
E. Development should integrate stormwater management infrastructure with open spaces, where possible.	Existing.
<b>Built Form (12.4) Performance Criteria</b>	
<b>General criteria for all development in the Alpine Region</b>	
A. Buildings should be efficient, well designed and successfully integrated with the surrounding landscape. This will be achieved by: i. ensuring building bulk, orientation and design contributes to the energy efficiency of buildings, particularly with respect to thermal comfort. ii. ensuring new buildings are located within existing disturbed areas to minimise impacts on vegetation and natural processes. iii. siting development within existing disturbed areas to limit clearing and the expansion of new development areas. iv. incorporating climate resilient design principles in new development. v. applying suitable rehabilitation and native landscaping. vi. incorporating preparedness for natural hazards and climate change into development design. ensuring development creates activated public domain spaces and provides safe and accessible pedestrian connections between buildings, appropriate for all seasons	The proposed internal maintenance works will not alter the apparent mass and bulk of the existing VT building.
B. Site earthworks must respond to local topography and geotechnical characteristics and be appropriate for the intended land use.	N/A no earthworks proposed.
<b>For village centres and public domain</b>	
A. Development should create an integrated streetscape where active frontages promote movement between the private and public realms.	N/A existing
B. Building entries should connect to an accessible (providing equitable access to all pedestrians) pedestrian network through design features, wayfinding, and landscape treatments	Complies



C. Development should integrate and provide public seating, shelter and lighting to contribute to increased activity and safety in the public realm	Existing
D. Development should provide human-scale buildings ensuring building envelopes allow adequate solar access and views, including ensuring significant views to natural features are protected	The proposed maintenance works will not change the design, scale and footprint of the existing building. Hence, there is no anticipated impact to the privacy of occupiers and users of other land. Furthermore the proposed works will not impact the solar access available to recreational users of the alpine resort.
E. Development should provide for year-round weather protection that reduces the impacts of wind and snow accumulation in winter and provides adequate shade in summer	Existing. The roof design of the VT building is appropriately sloped, and directs accumulated snow to fall away from entrances, establishing safe entry and exit into and out of the VT building.
F. Development should provide clearly defined and separate pedestrian and vehicle entries to minimise conflicts.	Existing
G. Development should allow for snow clearing and adequate interface with oversnow vehicles, where appropriate.	Existing

### 5.3.4 CHAPTER 13 TRANSPORT & INFRASTRUCTURE

<b>Transport Network (13.1) Performance Criteria</b>	
A. Transport infrastructure should integrate the public transport network with the existing road network by: i. ensuring public transport stops are strategically located and provide adequate all-weather shelter and accessibility. ii. minimising vehicle conflict with active transport and public transport routes	Existing
B. Development must provide operational access and egress for emergency services and occupants	Existing
C. Development should integrate active transport connections that promote movements between the Alpine resorts, where possible	Existing. The proposed works will not result in an increase of visitors to the resort.
D. New development must provide and integrate new technologies, such as electric vehicle charging and electronic checkpoints, where possible	N/A.
<b>Utilities, Services &amp; Infrastructure Performance Criteria</b>	
A. Development within the site must have access to water, wastewater, digital connectivity and telecommunications, energy and drainage infrastructure.	Complies. Existing
B. Utilities and services must be integrated with existing infrastructure and services, where possible.	Existing
C. Utilities and services should be integrated into road reserves, active transport corridors or the public domain, where possible.	Existing
D. Infrastructure and services must be designed to provide for the ultimate growth and development in Alpine Resorts.	Existing
E. Development should provide and integrate	Existing

water cycle management and renewable energy solutions into the design of buildings and structures, where possible	
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#### 5.4 OTHER APPROVALS RURAL FIRES ACT 1997

The subject site is identified as being in bushfire prone land, and hence as outlined in *Planning for Bushfire Protection – PBP 2019* (NSW RFS 2019) however, the works are internal works only. The external building surface will not be altered as a result of this application, hence a Bushfire Safety Authority is not required as part of this application.

## 5.5 ENVIRONMENTAL PLANNING AND ASSESSMENT ACT 1979 (203)

### 5.5.1 OBJECTS OF THE EP&A ACT

In determining a development application, a consent authority must take into consideration the matters referred to in Clause 4.15 (1) of the EP&A Act as are of relevance to the development:

4.15 – 1 (a) (i) the provisions of an environmental planning instrument
The applicable environmental planning instrument is State Environmental Planning Policy (Precincts—Regional) 2021.
4.15 – 1 (a) (ii) the provisions of any proposed instrument that is or has been the subject of public consultation under this Act and that has been notified to the consent authority (unless the Planning Secretary has notified the consent authority that the making of the proposed instrument has been deferred indefinitely or has not been approved)
None are applicable to the proposal.
4.15 – 1 (a) (iii) the provisions of any development control plan
None are applicable to the proposal.
4.15 – 1 (a) (iiia) the provisions of any planning agreement that has been entered into under section 7.4, or any draft planning agreement that a developer has offered to enter into under section 7.4
None are applicable to the proposal.
4.15 – 1 (a) (iv) the provisions of any Regulations (to the extent that they prescribe matters for the purposes of this paragraph)
None are applicable to the proposal.
4.15 – 1 (b) the likely impacts of that development, including environmental impacts on both the natural and built environments, and social and economic impacts in the locality
<p><b>NATURAL ENVIRONMENT:</b> The proposed works are not anticipated to have any negative impacts on the surrounding natural environment.</p> <p><b>BUILT ENVIRONMENT:</b> The proposed maintenance works will improve the condition of the VT building to ensure its longevity. The works will not involve any changes to the design, scale or footprint of the existing VT building. The proposal aims to retain the original alpine character and fabric of the Thredbo Village. Hence, the overall change to the built environment is considered of positive influence on the existing building and locality.</p> <p><b>SOCIAL AND ECONOMIC IMPACTS IN THE LOCALITY:</b> The proposed works have been designed to minimise any amenity impacts on the neighbouring buildings, they are not expected to generate any negative social or economic issues.</p>
4.15 – 1 (c) the suitability of the site for the development
The proposed maintenance works will improve the condition of the VT building and are deemed as suitable for the subject site.
4.15 – 1 (d) any submissions made in accordance with this Act or the regulations
The application does not require notification to adjoining landowners, as Kosciuszko Thredbo as the applicant, is also the Head Lessee.
4.15 – 1 (e) the public interest
The development proposal satisfies the objectives of the State Environmental Planning Policy (Precincts—Regional) 2021 and is considered positive in terms of the public interest.



## 6.0 CONCLUSION

### SUITABILITY OF THE SITE FOR DEVELOPMENT

The proposal complies with the relevant environmental planning instruments and policies. The Statement of Environmental Effects confirms that the site is suitable and capable of sustaining the proposed development, with no adverse impacts.

#### Submissions Made in Accordance with the Act or regulations:

Given the proposals minimal environmental impact it is unlikely to raise significant objection.

#### The Public Interest

The proposal is in the public interest as:

- It provides a sustainable land use.
- The proposal is suitable within the locality.
- The proposal is positive in terms of the amenity of the area.

## 7.0 SITE ENVIRONMENTAL MANAGEMENT PLAN (SEMP)

As detailed in the Statement of Environmental Effects, the proposed works will generate minimal impacts.

- No site clearing activity will occur through this project.
- No flow paths will be altered as a result of this development.
- Construction vehicles will enter the site via the existing site entry via the bridge from Friday Drive.
- Construction vehicles will park in the sites existing designated car parks.
- All construction materials will be stored in an appropriate location on site (to be determined by Kosciuszko Thredbo).

### DUST CONTROL

Works involving dust dispersion will use water spray to help keep material damp and dust down. Covers will be placed over waste storage areas and piles of excavated materials to prevent dust dispersion. When transporting materials that cause dust they will be dampened and covered before moving.

### LITTER CONTROL

Litter control around the site is the responsibility of all on site. A daily site clean up to reduce litter around the site and prevent any possible hazards it causes will be performed. It is the subcontractor's responsibility to leave the work area neat, clean and free of litter. Litter collected can be placed in bins or specified areas and disposed of at the local council tip.

### EMERGENCY PROCEDURES

In case of an emergency, the following key emergency response contacts are:

Organisation	Emergency Phone	Non-Emergency Phone
NSW Police	000	Adaminaby: 02 6456 2244
NSW Fire and Rescue	000	Cooma: 02 6452 2037
NSW Ambulance	000	Cooma: 02 9320 7777
Cooma Hospital	02 6455 3222	
National Parks and Wildlife Service (NPWS)/OEH	1800 629 104	Snowy Region: 6450 5600 Jindabyne: 6450 5555
Roads and Maritime Services	Traffic incidents & road conditions: 131 700 Road closures and special events: 132 701	
EPA Environment Line	131 555	
NRMA Road Service	13 21 32	

### NOISE CONTROL

Noise on a construction site can become a form of pollution to the local environment through the use of plant, machinery and tools. For protection of employees and visitors to the site they are issued with PPE including ear protection.

To reduce noise pollution from site the following procedures will be followed:

- All plant, machinery and tools will be maintained in good working order at all times;
- Work involving noisy tools or machinery to be used inside the building structure when possible;
- Strict hours of operation for each site will be implemented to reduce noise pollution to the surrounding areas
- In the instance of receiving a complaint in regards to noise levels immediate rectification will occur as far as practical.

### FUELS & CHEMICALS

No fuel or chemicals will be stored onsite during construction.

## CHEMICAL SPILL PREVENTION AND CONTAINMENT

The proposed development will not require the storage or use of any hazardous materials. However, if any petroleum products, trade waste, garbage and other noxious substances are required for the subject works, although unlikely to be required in any substantial amount, this will be appropriately stored off-site.

## INDIGENOUS HERITAGE

Should any material suspected of being an Aboriginal object become unearthed in the course of works associated with the proposed works, all work at that location shall cease immediately as per Section 90 of the *National Parks and Wildlife Act 1974*, and the Office of Environment and Heritage shall be contacted immediately to arrange for representatives to inspect the site.

## WASTE MANAGEMENT

Waste management is a crucial aspect to managing the impact of development in Kosciuszko National Park. No ground disturbance is proposed to occur within 40m of the Thredbo River, however caution must be taken when disposing of and handling waste material on site, to prevent any runoff of debris into the waterway. All waste generated from the proposed works, and the general construction waste and other waste associated with the repair work and construction of the new Mountain Bike Storage Facility will be disposed of in a skip bin, that will then be deposited to an external waste facility.

To ensure that waste is managed appropriately, the following controls and measures are to be adhered to:

- All litter generated on site is to be placed in small garbage bags. These bags are to be disposed of appropriately in a timely manner.
- A daily inspection shall be carried out to ensure the worksite is left in a rubbish free state.
- All employees shall be informed of the need to maintain a clean worksite.
- Site generated waste including garbage, grouting and mortar and excess stabilisation materials shall be collected and removed from the site.
- All loads of rubbish removed shall be securely covered to ensure no spillage.
- To the furthest extent possible efforts shall be made to reduce, reuse and recycle materials used onsite.
- The worksite shall be left in a tidy and rubbish free state upon completion of the Project.

## NOISE AND VIBRATION POLLUTION

The intended hours of construction have not yet been determined. Hours for construction will be determined in order to allow for construction on suitable days during appropriate hours to prevent any negative impacts on the amenity of visitors and staff.

## AIR POLLUTION

the construction of the proposed development is not expected to create any unnecessary air pollution.

## TRAFFIC MANAGEMENT

The overall principles for traffic management during demolition, excavation and construction phases of the development are:

- minimise effects on traffic movements and amenity;
- manage and control vehicular movements to and from the site;
- maintain traffic capacity at intersections;
- maintain existing on-street parking in the vicinity of the site where practical;
- maintain access to other properties adjacent to the site;
- maintain safety for workers;
- provide appropriate access to the site for, excavation and construction traffic; and
- manage and control vehicle activity in the vicinity of the site.